

Parking and Loading
Lake Havasu City – Development Code Excerpt

14.04.02 PARKING AND LOADING.

A. *Purpose and applicability.* It is the intent of this section to provide adequate parking and loading facilities to prevent street and traffic congestion while allowing for efficient use of land. The standards of this section shall apply to all development and redevelopment unless specifically excepted or modified by another provision of this Code.

B. *Exceptions and exemptions.*

1. Change in permitted use or structure.

a. Whenever the use of a property is changed to a new type of permitted or conditional use, parking and loading spaces required for the new use shall be provided in compliance with this section.

b. Whenever a structure is enlarged resulting in a net increase of gross floor area, seating capacity, dwelling units, or any other unit of measurement used to determine the required number of parking spaces, parking and loading spaces for the additional capacity shall be provided in compliance with this section.

c. Notwithstanding the provisions of divisions B.1.a. and B.1.b. above, a permitted use that does not meet the parking requirements of this section may be converted to another permitted use without full compliance with the required number of parking spaces provided:

i. The amount of parking available is at least 75% of the parking required for the new use in Table 4.02-1; and

ii. The applicant provides the maximum number of parking spaces able to be accommodated on the site while complying with all other provisions of this Code and without being required to remove or partially remove an existing structure.

2. Small non-residential lots. No off-street parking shall be required for any non-residential primary use on a lot in any mixed use district that is smaller than 5,000 square feet where no portion of the front lot line is located within 100 feet of a residential district.

3. Planned development.

a. The off-street parking requirements of this section shall serve as the standard from which to request different parking requirements for a PD-O (Planned Development Overlay) District.

b. Following approval of a PD-O District and general development plan, the parking requirements in those documents shall apply instead of the parking requirements in this section.

c. If an approved PD-O Zoning District general development plan or site plan is silent on any aspect of parking addressed by this section, the provisions of this section shall apply to that aspect of parking.

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4. Parking-in-common.

a. Development and redevelopment of lots subject to a parking-in-common agreement, on the effective date of this Code, shall comply with the terms of that agreement instead of the parking requirements in this section. A list of parking-in-common agreements known to the city, and the properties included in those agreements, is available from the Department.

b. The off-street parking requirements of this section shall serve as the standard from which to request different parking requirements for parking-in-common areas existing on the effective date of this Code, or for the creation of a new parking-in-common agreement after the effective date of this Code.

c. Owners of properties subject to an existing parking-in-common agreement may not physically separate or bar access to any part of the parking-in-common area in a manner not permitted by the parking-in-common agreement unless the owner first obtains modifies that agreement pursuant to § [14.05.04I](#).

C. *Parking requirements.*

1. Required parking spaces.

a. The minimum off-street parking requirements for the uses allowed by this Code are listed in Table 4.02-1.

b. When calculating parking spaces, requirements for a fraction of a parking space shall be ignored.

c. A reduction in the minimum off-street parking requirements may be permitted in accordance with division O. below.

d. For any uses not listed in Table 4.02-1, the required minimum number of off-street parking spaces shall be determined by the Zoning Administrator based on parking requirements for similar uses and any anticipated neighborhood and traffic congestion impacts.

2. Accessible parking. Within the requirements of Table 4.02-1, accessible parking shall be provided for all multi-family and non-residential uses as required by the City Building Code and the Americans with Disabilities Act (ADA).

3. Mixed uses. Where several uses occupy a single structure or parcel of land, the total requirement for off-street parking and loading shall be calculated based on division O.1.b. below.

TABLE 4.02-1: MINIMUM REQUIRED OFF-STREET PARKING	
SF = SQUARE FEET; GFA = GROSS FLOOR AREA	
LAND USE CATEGORY	STANDARD (ALL MINIMUMS UNLESS OTHERWISE LISTED)
RESIDENTIAL USES	
HOUSEHOLD LIVING	
DWELLING, SINGLE-FAMILY DETACHED	2 spaces/dwelling unit. At least one space shall be covered.
DWELLING, TWO-FAMILY DWELLINGS	

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TABLE 4.02-1: MINIMUM REQUIRED OFF-STREET PARKING	
SF = SQUARE FEET; GFA = GROSS FLOOR AREA	
LAND USE CATEGORY	STANDARD (ALL MINIMUMS UNLESS OTHERWISE LISTED)
DWELLING, SINGLE-FAMILY ATTACHED	1 space/dwelling unit
DWELLING, CO-HOUSING	
DWELLING, LIVE/WORK	
DWELLING, MULTI-FAMILY	R-UMS and MU-UMS districts: 1 parking space per unit. All required spaces must be covered. All other districts 1 space/dwelling unit containing 1 or fewer bedrooms; 1.5 spaces/dwelling unit containing more than 1 bedroom. Plus 1 space/5 dwelling units for visitor parking. At least 1 space per unit must be covered
DWELLING, VACATION RENTAL	See respective dwelling type
MANUFACTURED HOUSING PARK	2 spaces/dwelling unit. At least one space shall be covered.
GROUP LIVING	
CONTINUING CARE RETIREMENT COMMUNITY	1 space/3 habitable units. Plus guest parking at 10% of resident parking standards above. At least 50% of required spaces shall be covered.
RESIDENTIAL CARE FACILITY	1 space/3 beds design capacity. Plus guest parking at 10% of resident parking standards above. At least 50% of required spaces shall be covered.
PUBLIC AND INSTITUTIONAL USES	
COMMUNITY SERVICE	
CEMETERY OR MAUSOLEUM	No requirement
CLUBS, LODGES, AND PRIVATE MEETING HALLS	1 space/300 sf gfa
COLLEGE OR UNIVERSITY	1 space/ 500 sf gfa of classroom, office, research, and library area Auditorium must meet Theater/Auditorium parking standard
COMMUNITY CENTERS	1 space/500 sf gfa
DAY CARE FACILITY	1 space/800 sf gfa.
ELEMENTARY SCHOOL	1 space per classroom plus 1 space per 250 sf in largest auditorium or meeting space
SECONDARY SCHOOL (JUNIOR HIGH AND HIGH SCHOOL)	1 space per classroom plus 1 space per 200 sf in largest auditorium or meeting space
FUNERAL HOME OR MORTUARY	1 space 200 sf gfa in main assembly area
GOLF COURSE	6 spaces/hole or 1 space/400 sf of clubhouse area, whichever is greater
HOSPITAL	1 space/3 beds design capacity or 1 space/500 sf gfa, whichever is greater
LIBRARY, MUSEUM, OR GALLERY	1 space/1,000 sf gfa, but not less than 2 spaces
LONG-TERM MEDICAL CARE FACILITY	1 space/3 beds design capacity or 1 space/500 sf gfa, whichever is greater
OPEN SPACE, PARK, OR PLAYGROUND	No requirement
PUBLIC SAFETY FACILITY	1 space/300 sf gfa
RELIGIOUS FACILITY	1 space/6 fixed seats or per 200 sf gfa in the main assembly area, whichever is greater

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TABLE 4.02-1: MINIMUM REQUIRED OFF-STREET PARKING	
SF = SQUARE FEET; GFA = GROSS FLOOR AREA	
LAND USE CATEGORY	STANDARD (ALL MINIMUMS UNLESS OTHERWISE LISTED)
WATER-RELATED FACILITY	As determined by the zoning administrator based on proposed use, traffic impacts, multi-modal transportation options, site constraints, and surrounding land uses
UTILITIES & COMMUNICATIONS	
PUBLIC UTILITY FACILITY, MAJOR	No Requirement
PUBLIC UTILITY FACILITY, MINOR	
TELECOMMUNICATIONS FACILITY	
COMMERCIAL USES	
AGRICULTURE AND ANIMAL RELATED	
AGRICULTURE	No Requirement
ANIMAL CARE AND BOARDING	1 space/500 sf gfa
COMMUNITY GARDENS	No Requirement
PLANT NURSERY OR GARDEN SUPPLY	1 spaces/500 sf gfa
VETERINARY HOSPITAL	1 space/500 sf gfa
FOOD & BEVERAGE SERVICE	
BAR OR NIGHTCLUB	R-UMS and MU-UMS districts: 1 space/150 sf gfa
RESTAURANT	All other districts: 1 space/100 sf gfa No additional parking for approved outdoor eating areas
LODGING	
BED AND BREAKFAST	1 space/dwelling unit, plus 1 space/ rentable bedroom
HOTELS, MOTELS, AND CONFERENCE FACILITIES	2 spaces/3 guest rooms, plus 1 space/300 sf in all accessory uses including restaurants and meeting rooms; 10% of the required spaces shall be designed and dimensioned for RV parking
RESORT	As determined by the zoning administrator based on proposed use, traffic impacts, multi-modal transportation options, site constraints, and surrounding land uses
OFFICE	
BUSINESS, ART, OR VOCATIONAL SCHOOLS	1 space/400 sf gfa
OFFICES	R-UMS and MU-UMS districts: 1 space/500 sf gfa All other districts: 1 space/300 sf gfa
MEDICAL OFFICES AND CLINICS	R-UMS and MU-UMS districts: 1 space/400 sf gfa All other districts: 1 space/250 sf gfa
RESEARCH AND DEVELOPMENT LABORATORY	1 space/600 sf gfa
PERSONAL SERVICES	
PERSONAL SERVICES	R-UMS and MU-UMS districts: 1 space/400 sf gfa All other districts: 1 space/250 sf gfa
SELF-STORAGE (MINI-STORAGE)	2 spaces
RECREATION & ENTERTAINMENT	
ENTERTAINMENT, ADULT	1 space/300 sf gfa
INDOOR RECREATION OR ENTERTAINMENT	1 space/300 sf gfa
MOTORSPORTS FACILITY	As determined by the zoning administrator based on proposed use,

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LAND USE CATEGORY	STANDARD (ALL MINIMUMS UNLESS OTHERWISE LISTED)
OUTDOOR RECREATION OR ENTERTAINMENT	traffic impacts, multi-modal transportation options, site constraints, and surrounding land uses
SHOOTING RANGE	
STUDIO	1 space/400 sf gfa
THEATERS AND AUDITORIUM	1 space/6 seats or per 200 sf gfa in main auditorium, whichever is greater
RETAIL	Refer to Table 4.02-5 for drive-up/through facility stacking space requirements
ALCOHOL BEVERAGE SALES	1 space/300 sf gfa
BUILDING MATERIAL STORE	1 spaces/500 sf gfa
CONVENIENCE STORE (WITH FUEL SALES)	1 space/250 sf gfa
GENERAL RETAIL, SMALL	R-UMS and MU-UMS districts: 1 space/400 sf gfa All other districts: 1 space/300 sf gfa
GENERAL RETAIL, LARGE	1 space/400 sf gfa
MEDICAL MARIJUANA DISPENSARY OR INFUSION FACILITY	1 space/300 sf gfa
RETAIL, ADULT	1 space/300 sf gfa
RETAIL, BIG BOX	1 space/500 sf gfa
VEHICLES & EQUIPMENT	Refer to Table 4.02-5 for drive-up/through facility stacking space requirements
AIRPORT AND SUPPORT SERVICES	As determined by Lake Havasu City airport management
CAR WASH	No Requirement, but refer to Table 4.02-4 for drive-up/through facility stacking space requirements
HEAVY VEHICLE AND EQUIPMENT SALES, RENTAL, OR SERVICE	1 space/1,000 sf gfa
LIGHT VEHICLE SALES OR RENTAL	1 space/500 sf gfa in showroom
LIGHT VEHICLE SERVICE OR REPAIR	1 space/500 sf gfa
LIGHT WATERCRAFT SALES, RENTAL, OR SERVICE	1 space/500 sf gfa in showroom
MARINA	As determined by the zoning administrator based on proposed use, traffic impacts, multi-modal transportation options, site constraints, and surrounding land uses
MARINE EQUIPMENT SALES, RENTAL, SERVICE, AND STORAGE	1 space/500 sf gfa of sales, rental, and service area
PARKING FACILITY	No requirement
RECREATIONAL VEHICLE PARK	1 space/ RV site Plus guest parking at 10% of standard above.
VEHICLE AND FREIGHT TERMINALS	1 space/2,000 sf gfa
INDUSTRIAL USES	
COMMERCIAL SERVICES	
OUTDOOR STORAGE	No requirement
WAREHOUSE AND WHOLESALE DISTRIBUTION	1 space/2,000 sf gfa
MANUFACTURING & PROCESSING	
HEAVY INDUSTRY	
LIGHT INDUSTRY	1 space/1,000 sf gfa
MEDICAL MARIJUANA CULTIVATION FACILITY	
STORAGE OR USE OF HAZARDOUS MATERIALS	No Requirement

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TABLE 4.02-1: MINIMUM REQUIRED OFF-STREET PARKING	
SF = SQUARE FEET; GFA = GROSS FLOOR AREA	
LAND USE CATEGORY	STANDARD (ALL MINIMUMS UNLESS OTHERWISE LISTED)
WASTE & SALVAGE	
RECYCLING, LARGE COLLECTION OR LIGHT PROCESSING FACILITY	1 space/1,000 sf gfa
RECYCLING, SMALL COLLECTION FACILITY	No Requirement
ACCESSORY USES	
ACCESSORY AUTOMATED TELLER MACHINE (ATM)	No requirement; refer to Table 4.02-4 or drive-up/through facility stacking space requirements
ACCESSORY DRIVE-IN OR DRIVE-THROUGH	No requirement; refer to Table 4.02-4 for drive-up/through facility stacking space requirements
ACCESSORY DWELLING UNIT	1 space/dwelling unit in addition to primary dwelling requirement
ACCESSORY, OTHER	No requirement
TEMPORARY USES	
TEMPORARY CONSTRUCTION OFFICE OR YARD OR REAL ESTATE SALES OFFICE	2 spaces
TEMPORARY EVENT OR SALES OR OTHER CUSTOMARY USE	No requirement

D. *Parking alternatives.* In mixed-use and non-residential zoning districts, the Zoning Administrator may approve applications containing alternatives to the number of off-street parking spaces required by Table 4.02-1, if the application meets the following standards.

1. Shared parking.

a. *Location.* Shared parking shall be located within 600 feet of the primary entrance of all uses served.

b. *Reduction.* Where a shared parking area or facility meets the location requirement of division D.1.a. above, the off-site parking required for those uses may be reduced by the factors shown in Table 4.02-2. Off-street parking required shall be the sum of the 2 parking requirements for the 2 uses divided by the factors for that combination of uses. If more than 2 uses share a parking lot or structure, the required parking shall be calculated by applying Table 4.02-2 to the 2 uses with the largest parking requirements and then adding the required parking for the additional uses.

Table 4.02-2: Shared Parking Reduction Factors					
PROPERTY USE	RESIDENTIAL	PUBLIC AND INSTITUTIONAL	FOOD, BEVERAGE, AND INDOOR ENTERTAINMENT, LODGING, RELIGIOUS ASSEMBLY	RETAIL SALES AND PERSONAL SERVICES	OFFICE & OTHER COMMERCIAL
Residential					
Public and Institutional	1.1				
Food, Beverage, Indoor Entertainment,	1.1	1.2			

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Table 4.02-2: Shared Parking Reduction Factors					
PROPERTY USE	RESIDENTIAL	PUBLIC AND INSTITUTIONAL	FOOD, BEVERAGE, AND INDOOR ENTERTAINMENT, LODGING, RELIGIOUS ASSEMBLY	RETAIL SALES AND PERSONAL SERVICES	OFFICE & OTHER COMMERCIAL
Lodging, Religious Assembly					
Retail Sales and Personal Services	1.2	1.3	1.3		
Office & Other Commercial	1.3	1.5	1.7	1.2	

c. *Other sharing permitted.* As an alternative to the parking reduction factors in Table 4.02-2, the Zoning Administrator may allow the following reductions in required parking:

i. Up to 50% of the parking spaces required for food, beverage, and indoor entertainment uses located within 600 feet of a non-residential use that is not normally open, used, or operated during the same hours; and

ii. Up to 100% of parking spaces required for religious assembly uses located within 600 feet of a non-residential use that is not normally open, used, or operated during the same hours.

d. *Agreements.* If the Zoning Administrator determines that there is significant doubt that shared parking facilities will remain available for the uses they serve, and the loss of those shared parking spaces would cause significant neighborhood impacts or traffic congestion, the Zoning Administrator may require a written agreement assuring the continued availability of the shared parking spaces for the uses they serve. If required by the Zoning Administrator, the agreement shall be on a form approved by the city and shall be filed with the application for a building permit.

2. Bicycle facilities. For parking lots required to have more than 25 vehicle parking spaces, the required number of vehicle parking spaces may be reduced by 1 space for each bicycle parking space installed, up to a maximum 20% reduction of the number of required vehicle parking spaces.

b. *Design.*

i. *Location.* Bicycle parking facilities shall be:

- (A) Located near the main customer and employee entrances to the principal use;
- (B) Clearly designated;
- (C) Safely separated from vehicle maneuvering areas, and
- (D) Located on the ground level when in a parking structure.

ii. *Minimum dimensional standards.* Each bicycle parking space shall be at least:

- (A) Six feet long;

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(B) Two feet wide; and

(C) A minimum 5-foot wide access aisle is required beside or between each row of bicycle racks.

iii. *Materials.* Bicycle parking facilities shall be surfaced with all-weather material and maintained in a safe and neat condition that avoids dust.

iv. *Security.* Bicycle parking facilities shall be equipped with either a lockable enclosure (bicycle locker) or a permanent, secure, and stationary structure (bicycle rack) that supports the bicycle frame and to which the frame and both bicycle wheels can be locked.

3. Credit for motorcycle parking.

a. The Zoning Administrator may credit parking spaces for motorcycles or other small motorized vehicles against required off-street parking requirements in an amount of up to 5% of the standard vehicle spaces required.

4. Credit for public parking. Some or all of the required off-street parking spaces for a non-residential use may be waived by the Zoning Administrator if publicly owned off-street parking is located within a 600 foot walking distance from the main entrance of the proposed use, and the Zoning Administrator also determines that adequate parking spaces are available within the publicly owned parking area to accommodate the anticipated use.

5. Credit for on-street parking. The Zoning Administrator may credit on-street parking spaces against required off-street parking requirements if the on-street spaces are located within 600 feet of an entry of the building in which the use is located, if the Zoning Administrator determines that those parking spaces are frequently available for residents, patrons, or employees of the proposed use and structure.

6. Additional parking reductions. The Zoning Administrator may allow an additional reduction in the required number of parking spaces if the applicant submits a parking demand study, prepared in accordance with the city's guidelines, by a qualified parking or traffic consultant, documenting the basis for granting a reduced number of spaces, and the Zoning Administrator determines the study accurately reflects the parking demand for the use or structure.

E. *Parking design standards.* In mixed-use and special purpose districts, parking area and facility designs shall comply with the following standards:

1. Parking layout dimensions.

a. *Standard spaces.* All required parking spaces must be a minimum of 9 feet in width by 18.5 feet in length and must comply with the minimum dimensions for spaces and aisles shown in the following Table 4.02-4 and Figure 4.02-1.

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Table 4.02-4: Off-Street Parking Layout Dimensions						
	ONE-WAY DRIVE AISLE			TWO-WAY DRIVE AISLE		
Parking Angle	Stall Row Depth	Aisle Width	Stall Width	Stall Row Depth	Aisle Width	Stall Width
30°	17' 10"	12' 0"	9' 0"	13' 11"	18' 0"	9' 0"
45°	20' 6"	13' 0"	9' 0"	17' 4"	18' 0"	9' 0"
60°	21' 10"	18' 0"	9' 0"	19' 7"	18' 0"	9' 0"
90°				24' 0" for interior; 20' 0" for end of row	24' 0"	9' 0"

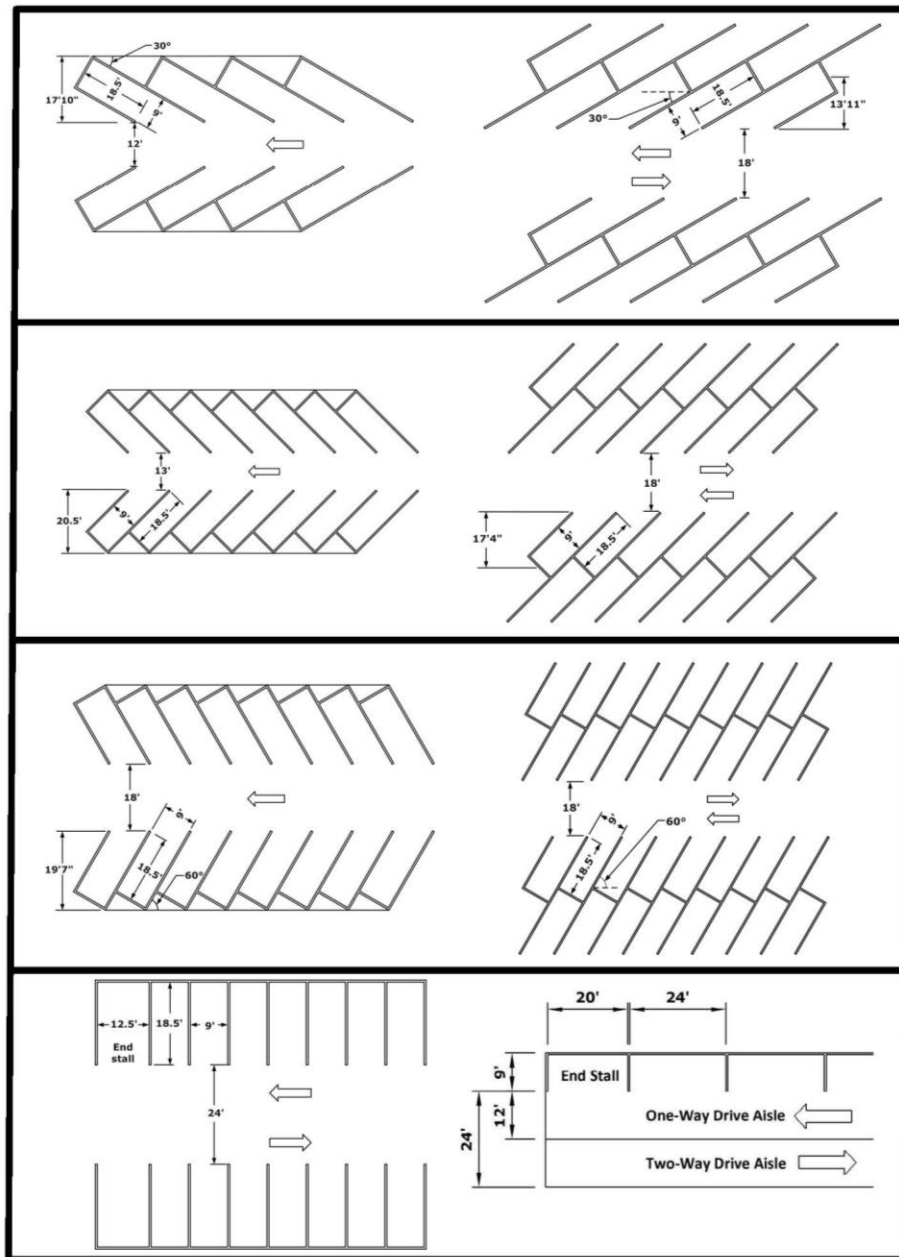


Figure 4.02-1: Parking Space and Aisle Dimensions

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b. *Accessible parking.* All required accessible parking spaces must comply with the following standards and minimum dimensions, as illustrated in Figure 4.02-2.

i. *Slope requirements.*

(A) A 5 foot square area cannot exceed 1:50 slope ratio in any direction if a raised sidewalk situation exists.

(B) Curb ramps are required with a raised sidewalk situation. Ramps cannot exceed a 1:12 slope ratio.

(C) An 11 foot space and 5 foot access aisle cannot exceed a 1:50 slope ratio in any direction.

ii. *Wheel stops.*

(A) Wheel stops are optional with curbed sidewalks.

(B) Wheel stops shall be setback a minimum of 3 feet from the edge of a curbed sidewalk.

iii. *Markings.*

(A) Accessible spaces shall be distinguished from standard parking spaces using different colors or physical barriers to outline the accessible spaces, and shall meet the requirements of Figure 4.02-2.

(B) The international handicap symbol shall mark the accessible space, with a blue background and yellow symbol.

(C) A minimum 3 inch outline striping is required. This striping shall be white on asphalt and traffic yellow on concrete. Measurements shall be taken from center to center.

(D) The access aisle adjacent to an accessible space shall be clearly marked with cross-hatching stripes spaced 2 feet apart on center.

(E) Access aisle must be clearly marked with diagonal stripes spaced 2 feet on center.

iv. *Signage.*

(A) The accessible space shall be identified by a standard regulatory sign (0.80 reflective aluminum with blue letters on a white background) meeting the requirements of Figure 4.02-3.

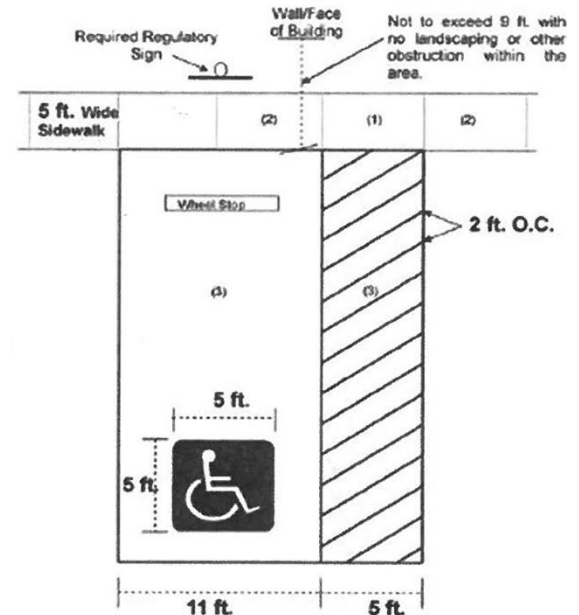


Figure 4.02-2:
Universal Handicap Space Standards

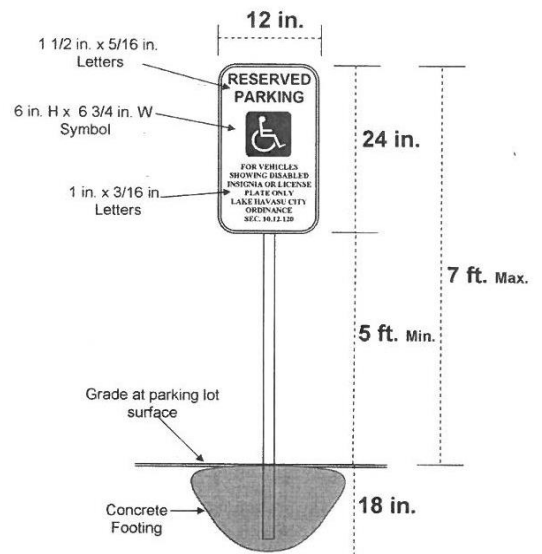


Figure 4.02-3:
Universal Handicap Space Signage

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(B) The sign shall be mounted on a custom or decorative post, or may be mounted on a wall if located within four feet of the edge of a sidewalk.

(C) No landscaping or other obstruction is permitted to block the view of the sign from the accessible parking space.

(D) The height of the sign shall not be more than 7 feet above grade level.

c. *Recreational vehicle spaces.* Recreational vehicle and boat trailer parking spaces shall be a minimum of 10 feet in width by 36 feet in length or 360 square feet in area.

d. *Motorcycle spaces.* Motorcycle spaces shall be a minimum of 5 feet in width by 12 feet in depth and appropriately identified.

2. Surfacing, marking, and drainage.

a. Areas used for the parking and maneuvering of any vehicle, boat, or trailer shall be improved with asphalt or concrete surfaces, or an alternate paving material approved by the Zoning Administrator.

b. Pervious or semi-pervious parking area surfacing materials may be approved alternatives, if the Zoning Administrator determines that they are appropriate for the city's climate and the durability required for their intended use. Permitted materials may include, but are not limited to grass, mulch, "grasscrete," ring and grid systems used in conjunction with grass seed or sod, permeable concrete or asphalt, porous or grid pavers, or recycle materials such as glass, rubber, used asphalt, brick, block, and concrete.

c. Vehicle storage areas may be surfaced with a permanent, dust-free pavement in lieu of concrete or asphalt pavement.

d. Required off-street parking spaces, drive aisles, and traffic flow indicators shall be clearly marked using reflectorized paint on the parking area pavement to maintain vehicular and pedestrian safety.

e. All on-site parking and loading areas shall include drainage facility designs that channel runoff to off-site drainage facilities as approved by the city.

3. Parking structures.

a. Parking structures are permitted up to the maximum height of the zoning district where they are located, or 48 feet, whichever is lower.

b. In the MU-CRW district, a parking structure must comply with the following setback requirements:

i. The parking structure shall be setback a minimum of 100 feet inland from the channel or 455 foot elevation line.

ii. The parking structure shall be setback a minimum of 10 feet from all right-of-way property lines.

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iii. Any parking structure height above 40 feet requires a setback ratio of 1 foot of building height for every 1/2 foot of setback measured from the 10 foot right-of-way setback required in division F.2. above.

iv. The design of the structure façade shall screen the lower 4 feet of each level of parking visible from any of the public streets bounding the block on which the parking structure is located, so that portions of automobiles below hood height are not visible from those public streets.

F. Residential standards.

1. The following standards apply to parking for one Single-Family or one Two-Family residential dwelling constructed in any residential district:

- a. Required parking spaces shall be hard surfaced, located off-street, and have access from a public street.
- b. Required parking spaces shall be a minimum of 9 feet in width by 18.5 feet in length if uncovered and 10 feet by 18.5 feet if covered.
- c. Required parking spaces shall not be located within the front setback of a lot

2. The following standards apply to parking for multiple-family residential dwellings constructed in the R-3, R-M, R-UMS, R-CHD, R-SGD Districts and any mixed-use district:

- a. Required parking spaces shall be hard surfaced, located off-street, and have access from a public street
- b. Required parking spaces shall be a minimum of 9 feet in width by 18.5 feet in length if uncovered and 10 feet by 18.5 feet if covered.
- c. Driveways and parking access ways shall not occupy more than 50% of the required front setback area
- d. Required parking spaces shall not be located within the front setback of a lot.
- e. The parking area shall be designed to prevent a vehicle from backing onto, into, or over a public right-of-way.

G. Parking of oversized vehicles. The following standards apply to all properties in the residential and mixed use zone districts.

1. **Oversized vehicles.** Vehicles with a rated chassis capacity in excess of 2 tons, a gross vehicle weight in excess of 10,500 pounds, or any vehicle with more than 2 axles, may not be parked on any lot, except for the purpose of loading or unloading of goods, except as provided in division H.2. below.

2. **Recreational vehicles.** A recreational vehicle exceeding 10,500 pounds that is owned by the lot owner, or the lessee of the lot, may be parked on the lot if the property is developed with a primary use (e.g. a house).

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H. *Drive-through vehicle stacking.* The following standards apply to all properties with a drive-through facility.

1. Stacking space requirements.

Table 4.02-5: Drive-Through Stacking Space Requirements		
ACTIVITY	MINIMUM STACKING SPACES (PER LANE)	MEASURED FROM
Bank, Financial Institution, or ATM	3	Teller or Window
Restaurant/Retail Store	3	Pick-Up Window
Full Service Car Wash	3	Outside of Washing Bay
Self-Service Car Wash	1	Outside of Washing Bay
Other	Determined by the Zoning Administrator based on anticipated need to avoid traffic congestion on adjacent streets	

2. Location and design of stacking lanes.

a. Audible electronic devices such as loudspeakers, automobile service order devices, and similar instruments shall not be audible beyond the property line of the site.

b. Drive-through stacking lanes shall have a minimum width of 10 feet.

c. Stacking lanes shall be set back 15 feet from each right-of-way.

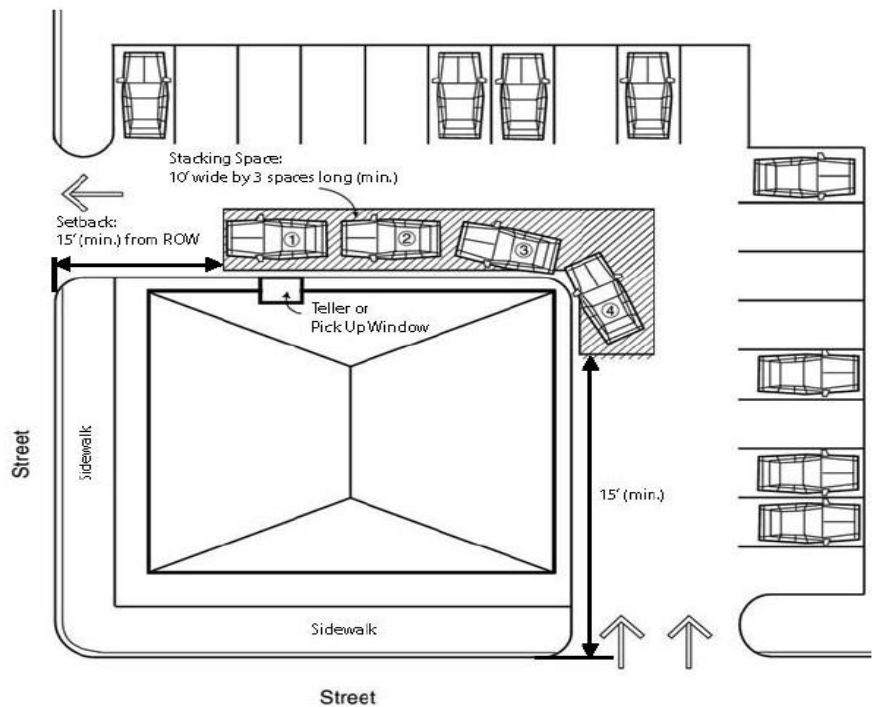


Figure 4.02-4: Stacking Lanes

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I. Off-street loading requirements.

1. Minimum requirements for off-street loading space.

- a. A required off-street loading space shall not be less than 10 feet in width by 25 feet in length and shall have an unobstructed height of 14 feet.
- b. Off-street loading space shall be provided as set forth in Table 4.02-6 below, except as provided in division J.2. below.

Table 4.02-6: Off-Street Loading Space Standards For Buildings Exceeding 20,000 sq. ft. of gross floor area	
TYPE OF USE OR FACILITY	OFF-STREET LOADING REQUIREMENT
Office or Lodging Personal services and repair Retail store (large)	1 space for each 50,000 sf of gross floor area in the structure, or part thereof, up to a maximum of 3 spaces
Building material sales Garden material sales Furniture and floor covering sales Industrial Services and Manufacturing Light vehicle sales, rental, storage, repair, or service Heavy vehicle and equipment sales, rental, storage, repair, or service Wholesaling	1 space for each 50,000 sf of gross floor area in the structure, or part thereof, up to a maximum of 2 spaces
Bus and Truck Terminals	As determined by the Zoning Administrator to accommodate the maximum number of buses or trucks to be stored or loading/unloading at the terminal at any one time

2. Existing structures. If the aggregate gross floor area of any existing building is increased by more than 50%, off-street loading space shall be provided in accordance with Table 4.02-6 above, to the extent required for the original gross floor area and its expansion. If the aggregate gross floor area of any existing building is increased by 50% or less, then the new off-street loading space shall be provided to the extent required for the expansion only.

3. Design and use of off-street loading areas.

- a. Off-street loading space shall be located on the same lot occupied by the use served and shall be accessible from a public street or alley.
- b. Off-street loading space shall not be occupied by or considered as any part of the required off-street parking areas.
- c. No portion of an off-street loading space shall be located within any fire lane required by city ordinance or within aisles, driveways, or maneuvering areas necessary to provide reasonable access to any parking space.

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d. Off-street loading space and the aisles or driveways leading to them shall not be used for the sale, repair, dismantling, or servicing of any vehicles, equipment, materials or supplies.

e. Any off-street loading area located within 100 feet of the boundary of a residential zone district shall be screened from view from the district by a masonry wall at least 8 feet in height of a color matching 1 of the primary colors used on the primary façade of the building.

J. *Landscaping and screening.* All parking areas and facilities shall comply with the screening and landscaping requirements of §[14.04.04](#).